



AMERICAN SOCIETY OF HIGHWAY ENGINEERS

National Project of the Year Award

OFFICIAL ENTRY FORM

AWARD CATEGORY (Check One): Under \$20 Million Over \$20 Million

SPONSORING REGION (Check One):

- | | | |
|---|--|---|
| <input checked="" type="checkbox"/> Northeast | <input type="checkbox"/> Great Lakes | <input type="checkbox"/> Northwest |
| <input type="checkbox"/> Mid-Atlantic | <input type="checkbox"/> North Central | <input type="checkbox"/> Rocky Mountain |
| <input type="checkbox"/> Southeast | <input type="checkbox"/> South Central | <input type="checkbox"/> Southwest |

CONTACT INFORMATION FOR SUBMITTING REGION:

Contact Name: Scott R. Eshenaur ASHE Region Position: Judging Committee Chairperson
Phone (Office): 717.790.9565 Phone (Mobile): 717.580.8426 E-Mail Address: sreshenaur@modjeski.com
ext. 10422

PROJECT INFORMATION:

ENTERING AGENCY/COMPANY'S NAME: McCormick Taylor
PROJECT NAME: Route 44 Truck Bypass and DuPont Port Access TYPE: <20 Million
PROJECT LOCATION: Route 44 Truck Bypass and DuPont Port Access
CITY: Township of Greenwich COUNTY: Gloucester STATE: NJ
FINAL CONSTRUCTION COST: \$9.8 Million BUDGETED CONSTRUCTION COST: \$9.8 Million
PROJECT COMPLETION DATE: July 2021

PROJECT ASHE SECTION: ASHE NCNJ / SNJ ASHE SECTION CONTACT NAME: Stella Karcnik
PHONE (OFFICE): 609 512 3475 PHONE (MOBILE): 609 751 6865 E-MAIL: Stella.Karcnik@wsp.com

PROJECT TEAM:

OWNER

Name: Gloucester County Improvement Authority
Contact Person: Mario Christina
Address: 109 Budd Blvd, Woodbury, NJ 08096
Phone: 856.848.4002

DESIGNER

Name: McCormick Taylor, Inc.
Contact Person: Vittorio Anepete, PE
Address: 700 East Gate Dr, Suite 201, Mt Laurel, NJ, 08054
Phone: 856.206.5300

CONTRACTOR

Name: R.E. Pierson Construction
Contact Person: Mike Capasso
Address: 426 Swedesboro Rd, Pilesgrove, NJ 08098
Phone: 856.769.8244

Entry Form Completed By: Stella Karcnik, ASHE NC NJ POY Comitee chair Date: 2/8/2023



Project Description

Role of McCormick Taylor and Other Consultants

McCormick Taylor was responsible for the Concept Development, Preliminary Engineering, and Final Design associated with the construction of a new 0.60-mile truck bypass roadway in western Gibbstown to reduce truck traffic traveling through residential communities and increase quality of life and safety. As part of Final Design phase services, KMA Consulting Engineers, Inc. provided supplemental and right-of-way surveys, subsurface utility engineering (SUE), geotechnical engineering, and pavement design. Dresdner Robin performed a hazardous waste investigation and prepared a Materials Management Plan. RGA, Inc. performed Phase 1A and Phase 1B archaeological surveys and Amy Greene Environmental (now Davey Resource Group) performed an ecology study to support the EO215.

Complexity

During the Local Concept Development (LCD) Study, McCormick Taylor evaluated existing and future no-build conditions within the project area, prepared a project purpose and need statement, developed and analyzed alternatives for a Route 44 truck bypass that would also provide access to the Repauno site, and selected a Preliminary Preferred Alternative (PPA). During the Preliminary Engineering and Final Design Phases, this PPA was advanced to construction.

McCormick Taylor also provided extensive utility coordination and developed schemes of accommodation for six impacted companies, including Sunoco Pipeline, South Jersey Gas, Atlantic City Electric, AT&T, Greenwich Township Public Works, and Delaware River Partners. Detailed coordination was required to identify and avoid impacts to a not-in-service hydrogen line and a well that provided water to the entire redevelopment site. The project included preparation of jurisdictional limit maps, design exception approval, and highway lighting design. Structural design included the design of steel sheet pile retaining walls to limit the lateral extent of filling, concrete barrier with moment slabs, and revised slope protection at the existing Route 44 overpass. Lightweight fill was proposed on the existing culvert over Sand Ditch to avoid increasing the dead load on the structure.

The project also included the design of a new at grade railroad crossing. McCormick Taylor coordinated with NJDOT Bureau of Railroad Engineering Services and Conrail to obtain approval of the proposed crossing and warning devices. McCormick Taylor scheduled the Railroad Diagnostic Team Meeting on site with NJDOT, the Assistant County Engineer, Local Emergency Management Officials, Conrail, and DuPont Port Owners. The Memorandum of Record and the Permissive Order were obtained from NJDOT.



New Application of Existing Technologies/Originality/Innovation

The PPA was selected through a careful balance of providing desirable design elements (e.g. design speed, cross-section, curve radii, profile, etc.) and minimizing project impacts and costs. Major constraints driving the alignment of the bypass included impacts to residential Gibbstown, environmental resources, existing utilities, and railroad infrastructure. The PPA was able to maintain all existing structures.

McCormick Taylor was able to modify the slope protection of the existing Route 44 Bridge over Conrail to pass the northbound ramp under the first span of the bridge. The team was also able to utilize lightweight fill over the existing culvert over Sand Ditch to provide a widened roadway section with concrete barriers and moment slabs without impacting the culvert.

Social/Economic Considerations

The former DuPont site is being redeveloped into the Repauno Port & Rail Terminal and will play a vital role in the future economic success of South Jersey and the region. The port is located on the Delaware River and is served by Conrail with access to CSX and Norfolk Southern. The addition of the bypass provides easy access to I-295 and I-95 and safely diverts truck traffic from the surrounding residential neighborhoods improving the health, safety, and welfare of the residents.

Safety

Without a truck bypass, traffic traveling to/from the DuPont site was forced to go through a residential neighborhood along Repauno Avenue to access Route 44 or Democrat Road. Route 44 (Broad Street) is essentially Gibbstown's downtown Main Street. An analysis of future no-build conditions (without construction of a bypass) indicated that the Route 44 and Repauno Avenue intersection would operate at level of service (LOS) F with excessive delays (approximately four minutes). Traffic operations along the surrounding roadway network would also be degraded, with the unsignalized intersection at Swedesboro Road and Democrat Road (currently a flashing beacon) anticipated to operate at LOS F due to the trips generated by the proposed port redevelopment. Ten right-angle crashes were reported at this intersection during the most recent three-year period. Therefore, safety was also a concern with the increase in traffic volumes, especially with the expected heavy truck traffic. The proposed bypass permitted all port bound traffic from interstate and arterial connections to the south to be diverted to the port prior to entering downtown Gibbstown.



Aesthetics and Sustainable Features

The proposed alignment and profile increase utilized steel sheet piling retaining walls to limit tree clearing and contain impacts to the adjacent wetlands. The NJDEP Natural Heritage Program (NHP) identified a nest and foraging habitat for the State Endangered Bald Eagle within the immediate vicinity of the project. Sand Ditch also appeared to support populations of potential warm water forage fish species. The project maintained existing structures limiting additional disturbance, wetland, riparian, and ecological impacts around Sand Ditch and throughout the project. Much of the surrounding vegetation was maintained providing a buffer to adjacent residential areas. While much of the alignment is buffered from residential views, steel sheet piling was coated with black epoxy and capped with concrete coping to provide a clean finished appearance.

Meeting and Exceeding Owner's/Client's Needs

Gloucester County Improvement Authority, Gloucester County Engineering Department, and NJDOT Local Aid were engaged throughout Concept Development, with selection of the PPA, and through Preliminary Engineering and Final Design. The PPA was selected as a cost-effective solution that limited impacts to the existing structures along Route 44, while minimizing utility and environmental impacts. The project was completed on schedule and under original cost estimates from Concept Development. The Preliminary and Final Design phases were combined to accelerate the design of the project. The entire project from the beginning of Concept Development through completion of Construction was completed within four years.

**Total Project Construction
Budget Cost:**
\$9.8 Million

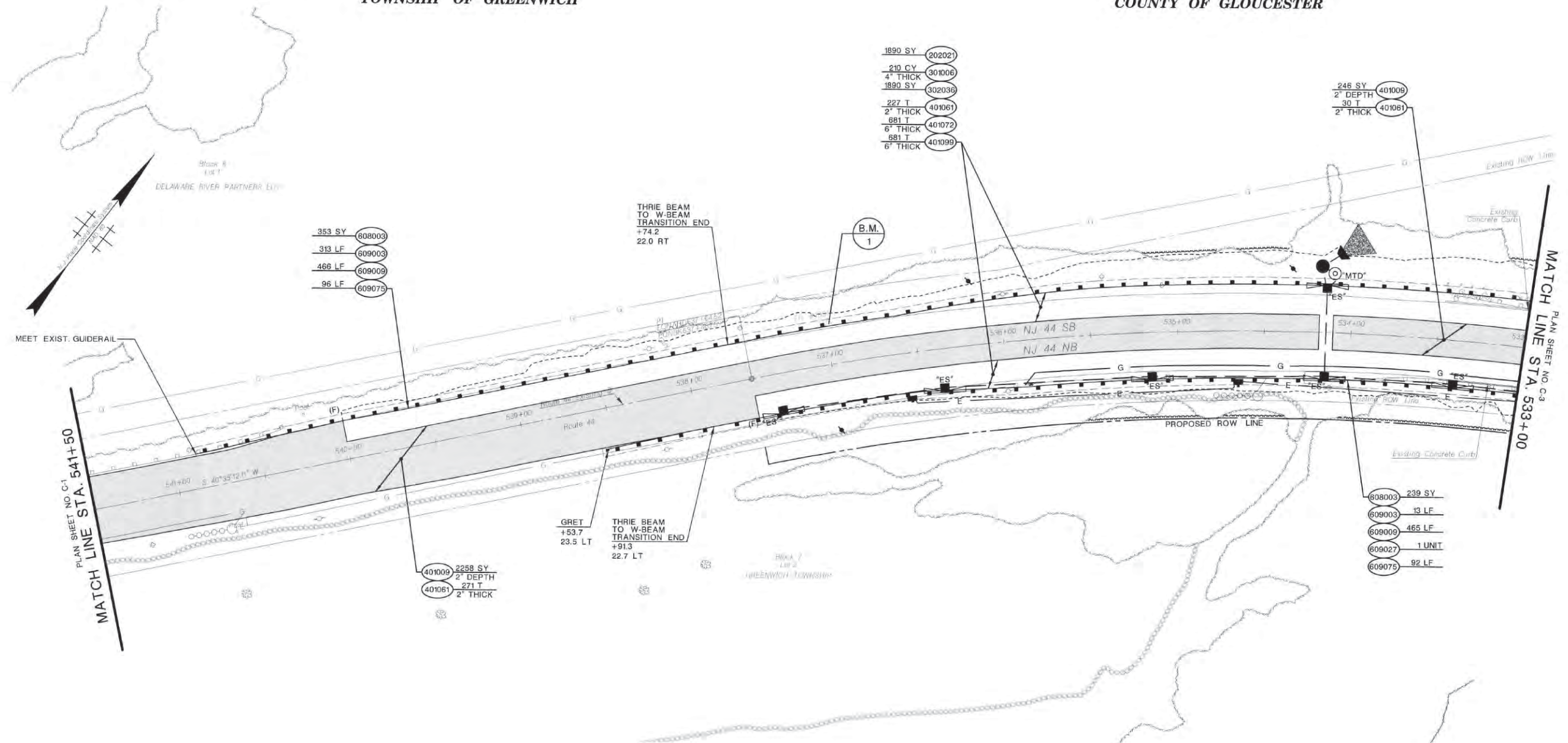
**Entrant's Portion of Total
Budget Cost:**
\$829,000

**Project Scheduled Date of
Completion:**
May 2021

**Total Project Construction
Actual Cost:**
\$9.8 Million

**Entrant's Portion of Total
Actual Cost:**
\$829,000

**Project Actual Date of
Completion:**
July 2021



- 353 SY (608003)
- 313 LF (609003)
- 466 LF (609009)
- 96 LF (609075)

- 1890 SY (202021)
- 210 CY (301006)
- 4" THICK (302036)
- 1890 SY (302036)
- 227 T (401061)
- 2" THICK (401061)
- 681 T (401072)
- 6" THICK (401072)
- 681 T (401099)
- 6" THICK (401099)

- 246 SY (401009)
- 2" DEPTH (401009)
- 30 T (401081)
- 2" THICK (401081)

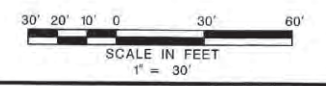
- 401009 2258 SY
- 401061 271 T
- 401061 2" THICK

- 608003 239 SY
- 609003 13 LF
- 609009 465 LF
- 609027 1 UNIT
- 609075 92 LF

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 REV. DATE: 11/02/2019
 SCALE: 30,0000 / 1"



ITEM NO.	TO BE CONSTRUCTED	CONTRACT QUANTITY
202021P	REMOVAL OF PAVEMENT	1890 SY
301006P	SUBBASE	210 CY
302036P	DENSE-GRADED AGGREGATE BASE COURSE, 6" THICK	1890 SY
401009P	HMA MILLING, 3" OR LESS	2504 SY
401061M	HOT MIX ASPHALT 12.5 M E SURFACE COURSE	528 T
401072M	HOT MIX ASPHALT 12.5 M 64 INTERMEDIATE COURSE	681 T
401099M	HOT MIX ASPHALT 25 M 64 BASE COURSE	681 T
608003P	NONVEGETATIVE SURFACE, HOT MIX ASPHALT	592 SY
609003M	BEAM GUIDE RAIL	326 LF
609009M	MODIFIED THRIE BEAM GUIDE RAIL	931 LF
609027M	TANGENT GUIDE RAIL TERMINAL	1 U
609075M	REMOVAL OF BEAM GUIDE RAIL	168 LF



GLOUCESTER COUNTY IMPROVEMENT AUTHORITY

CONSTRUCTION PLAN

ROUTE 44 TRUCK BYPASS
AND DUPONT PORT ACCESS

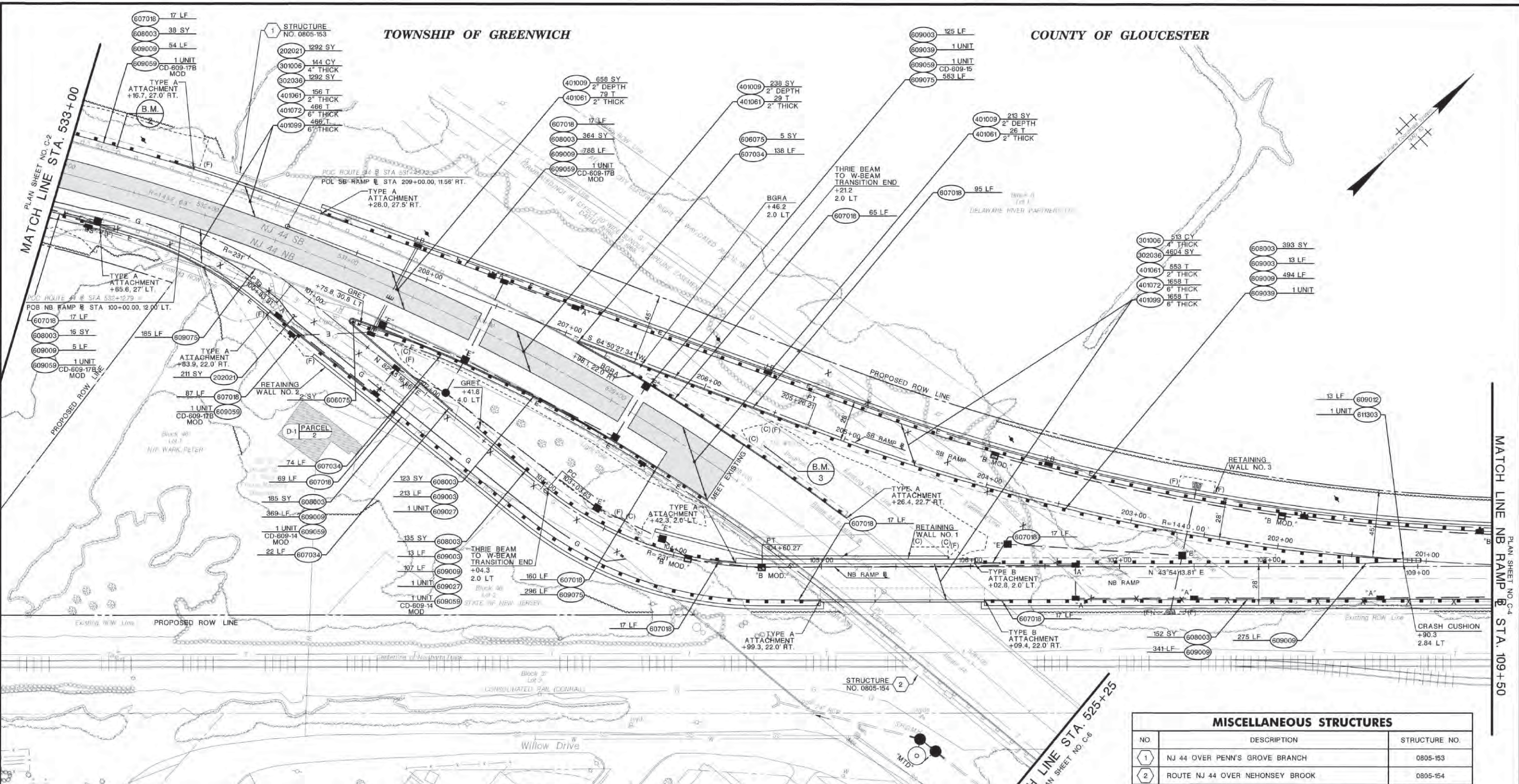
CERTIFICATION OF AUTHORIZATION NO. 24GA28044300
Anthony M. Dimaggio, Jr. DATE: 12/2/2019
 ANTHONY M. DIMAGGIO, JR., P.E.
 NEW JERSEY PROFESSIONAL ENGINEER LICENSE NO. 24GE04136290

TOWNSHIP OF GREENWICH

COUNTY OF GLOUCESTER

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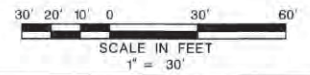
MCCORMICK TAYLOR



ITEM NO.	TO BE CONSTRUCTED	CONTRACT QUANTITY
201033M	DEMOLITION (D-1), PARCEL (2)	1 U
202021P	REMOVAL OF PAVEMENT	1603 SY
301006P	SUBBASE	657 CY
302038P	DENSE-GRADED AGGREGATE BASE COURSE, 6" THICK	5896 SY
401009P	HMA MILLING, 3" OR LESS	109 SY
401061M	HOT MIX ASPHALT 12.5 M E SURFACE COURSE	843 T
401072M	HOT MIX ASPHALT 12.5 M 64 INTERMEDIATE COURSE	2124 T
401099M	HOT MIX ASPHALT 25 M 64 BASE COURSE	2124 T
606075P	CONCRETE ISLAND, 4" THICK	7 SY
607018P	9" X 16" CONCRETE VERTICAL CURB	595 LF
607034P	9" X 14" CONCRETE VERTICAL CURB	233 LF
608003P	NONVEGETATIVE SURFACE, HOT MIX ASPHALT	1406 SY
609003M	BEAM GUIDE RAIL	364 LF
609009M	MODIFIED THREE BEAM GUIDE RAIL	2433 LF
609012M	MODIFIED THREE BEAM GUIDE RAIL, DUAL FACED	13 LF
609027M	TANGENT GUIDE RAIL TERMINAL	2 U
609039M	BEAM GUIDE RAIL ANCHORAGE	2 U
609059M	APPROACH GUIDE RAIL TRANSITION TL-3	7 U
609075M	REMOVAL OF BEAM GUIDE RAIL	1064 LF
611303M	CRASH CUSHION, COMPRESSIVE BARRIER, TYPE 2, WIDTH MEDIUM	1 U

ITEM NO.	DESCRIPTION	DESIGN SPEED	ROUTE & APPROXIMATE STATION	PRODUCT	FOUNDATION	BACKUP SYSTEM
611303M	CRASH CUSHION, COMPRESSIVE BARRIER, TYPE 2, WIDTH MEDIUM	45 MPH	NB RAMP @ STA. 109+90.3	QUADGUARD II 3 BAY 36" WIDE 12' 11" LENGTH	8" REINFORCED CONCRETE PAD	TENSION STRUT BACKUP
611303M	CRASH CUSHION, COMPRESSIVE BARRIER, TYPE 2, WIDTH MEDIUM	45 MPH	NB RAMP @ STA. 109+90.3	SCI170GM 36" WIDE 19' 5" LENGTH	8" CONCRETE PAD	SELF SUPPORTING
611303M	CRASH CUSHION, COMPRESSIVE BARRIER, TYPE 2, WIDTH MEDIUM	45 MPH	NB RAMP @ STA. 109+90.3	UNIVERSAL TAU II 4 BAY 36" WIDE 15' 9" LENGTH	8" CONCRETE PAD	WIDE FLANGE BACKSTOP

NO.	DESCRIPTION	STRUCTURE NO.
1	NJ 44 OVER PENN'S GROVE BRANCH	0805-153
2	ROUTE NJ 44 OVER NEHONSEY BROOK	0805-154



GLOUCESTER COUNTY IMPROVEMENT AUTHORITY

CONSTRUCTION PLAN

ROUTE 44 TRUCK BYPASS AND DUPONT PORT ACCESS



CERTIFICATION OF AUTHORIZATION NO. 24GA28044300
 ANTHONY M. DIMAGGIO, JR., P.E.
 NEW JERSEY PROFESSIONAL ENGINEER LICENSE NO. 24GE04182800

DATE: 12/2/2019

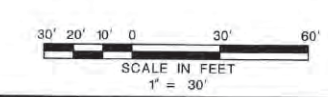
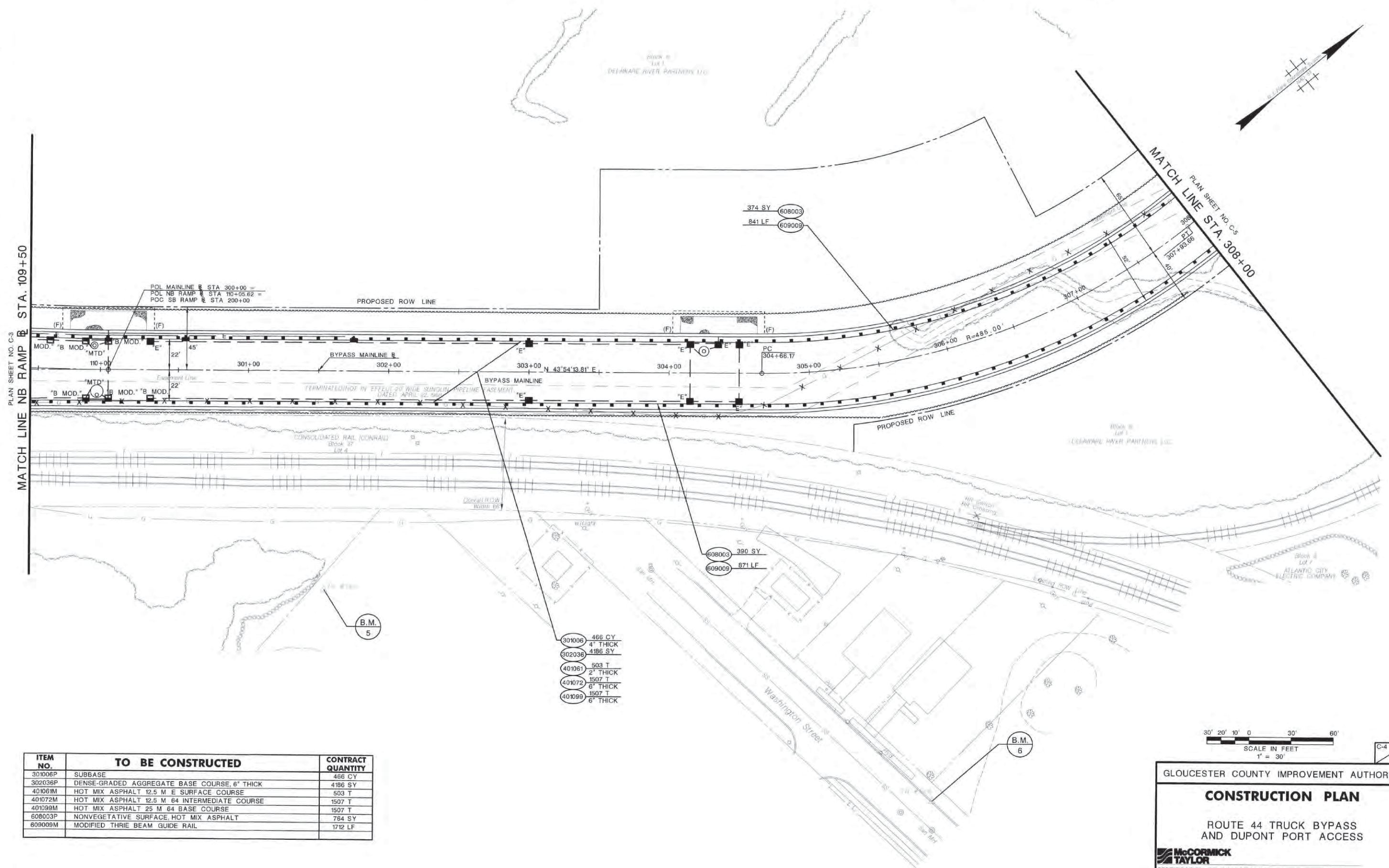
B.M. 2 - REBAR WITH ALUMINIUM CAP ROUTE 44 EXISTING B.L. STA. 532+69.40, 25.73' RT., ELEV. 11.772
 B.M. 3 - REBAR WITH ALUMINIUM CAP SB RAMP B.L. STA. 205+49.06, 23.30' LT., ELEV. 29.096

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 11/20/2019
 30.0000 / in.

**McCormick
Taylor**

PLAN SHEET NO. C-3
 MATCH LINE NB RAMP @ STA. 109+50

MATCH LINE STA. 308+00
 PLAN SHEET NO. C-5



ITEM NO.	TO BE CONSTRUCTED	CONTRACT QUANTITY
301006P	SUBBASE	466 CY
302036P	DENSE-GRADED AGGREGATE BASE COURSE, 6" THICK	4186 SY
401061M	HOT MIX ASPHALT 12.5 M E SURFACE COURSE	503 T
401072M	HOT MIX ASPHALT 12.5 M 64 INTERMEDIATE COURSE	1507 T
401099M	HOT MIX ASPHALT 25 M 64 BASE COURSE	1507 T
608003P	NONVEGETATIVE SURFACE, HOT MIX ASPHALT	764 SY
609009M	MODIFIED THRIE BEAM GUIDE RAIL	1712 LF

GLOUCESTER COUNTY IMPROVEMENT AUTHORITY

CONSTRUCTION PLAN

ROUTE 44 TRUCK BYPASS AND DUPONT PORT ACCESS



CERTIFICATION OF AUTHORIZATION NO. 24GA28044300

DATE: 12/2/2019
 ANTHONY M. DIMAGGIO, JR., P.E.
 NEW JERSEY PROFESSIONAL ENGINEER LICENSE NO. 24GE04138200

B.M. 5 - REBAR WITH ALUMINIUM CAP MAINLINE B.L. STA. 301+54.62, 165.88' RT., ELEV. 1.901
 B.M. 6 - REBAR WITH ALUMINIUM CAP MAINLINE B.L. STA. 305+41.10, 313.47' RT., ELEV. 3.660

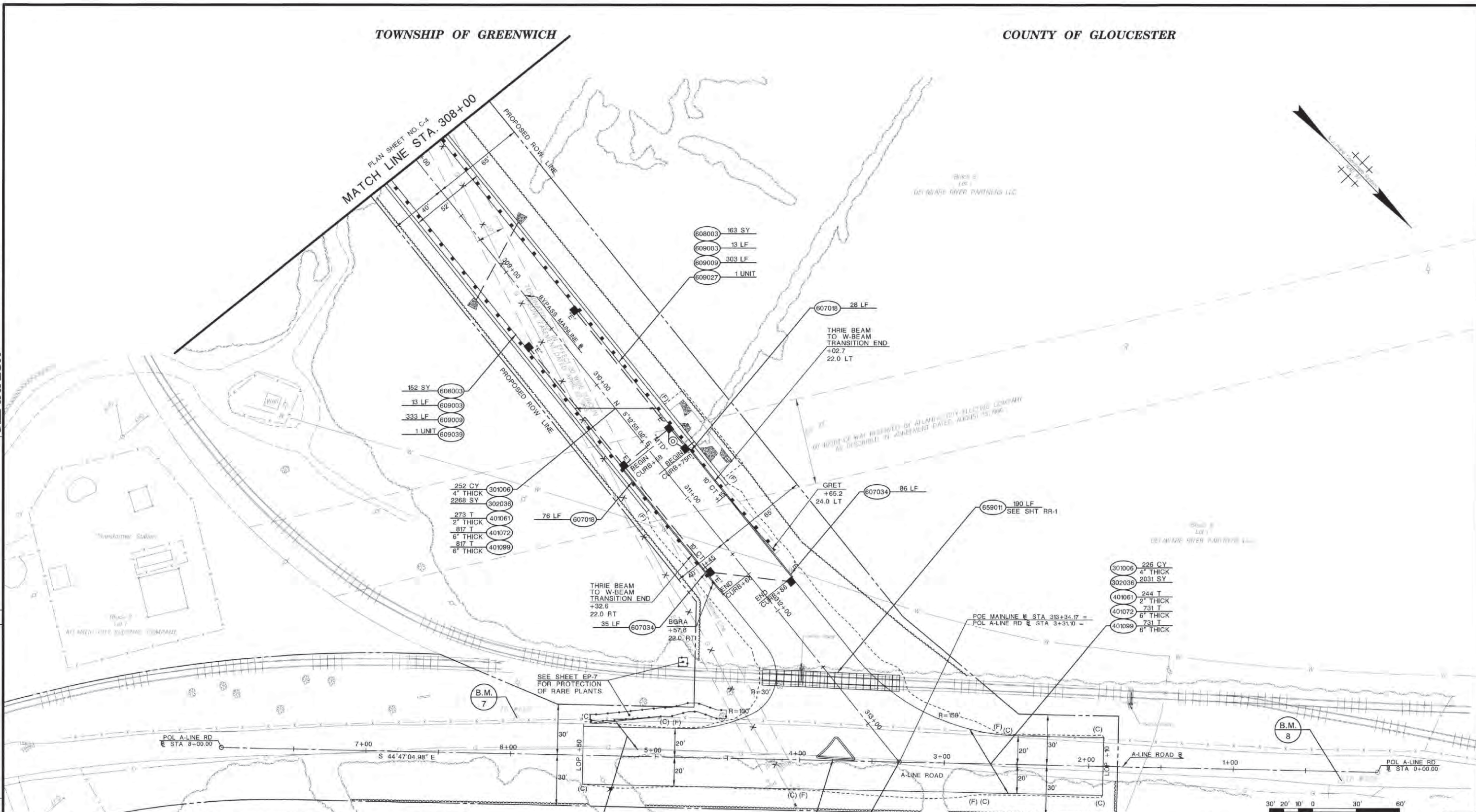
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C-8

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REVISID 08/22/2019 - PER INDEF. T&E COMMENTS



ITEM NO.	TO BE CONSTRUCTED	CONTRACT QUANTITY
202021P	REMOVAL OF PAVEMENT	803 SY
301006P	SUBBASE	478 CY
302036P	DENSE-GRADED AGGREGATE BASE COURSE, 6" THICK	4299 SY
401061M	HOT MIX ASPHALT 12.5 M E SURFACE COURSE	517 T
401072M	HOT MIX ASPHALT 12.5 M 64 INTERMEDIATE COURSE	1548 T
401099M	HOT MIX ASPHALT 25 M 64 BASE COURSE	1548 T
606075P	CONCRETE ISLAND, 4" THICK	19 SY
607018P	9" X 16" CONCRETE VERTICAL CURB	104 LF
607030P	12" X 13" CONCRETE SLOPING CURB	68 LF
607034P	9" X 14" CONCRETE VERTICAL CURB	121 LF
609003P	NONVEGETATIVE SURFACE, HOT MIX ASPHALT	315 SY
609003M	BEAM GUIDE RAIL	26 LF
609009M	MODIFIED THRIE BEAM GUIDE RAIL	636 LF
609027M	TANGENT GUIDE RAIL TERMINAL	1 U
609039M	BEAM GUIDE RAIL ANCHORAGE	1 U
659011M	HEAVY DUTY CONCRETE AT GRADE RAILROAD CROSSING SYSTEM	190 LF

GLOUCESTER COUNTY IMPROVEMENT AUTHORITY

CONSTRUCTION PLAN

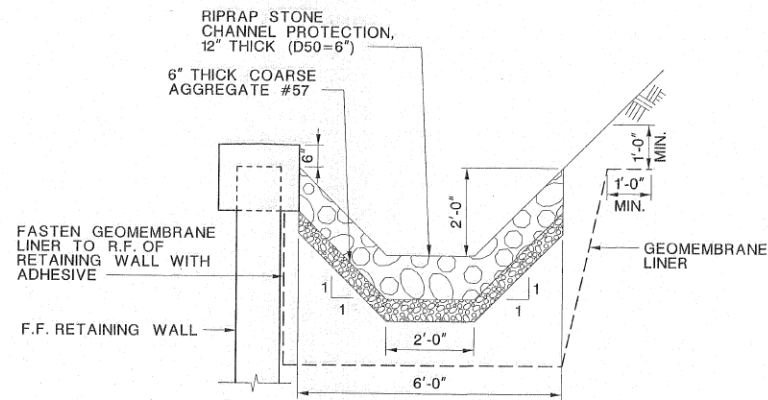
ROUTE 44 TRUCK BYPASS AND DUPONT PORT ACCESS

McCormick Taylor
 CERTIFICATION OF AUTHORIZATION NO. 24GA28044300
 DATE: 12/2/2019
 ANTHONY M. DIMAGGIO, JR., P.E.
 NEW JERSEY PROFESSIONAL ENGINEER LICENSE NO. 24E04136200

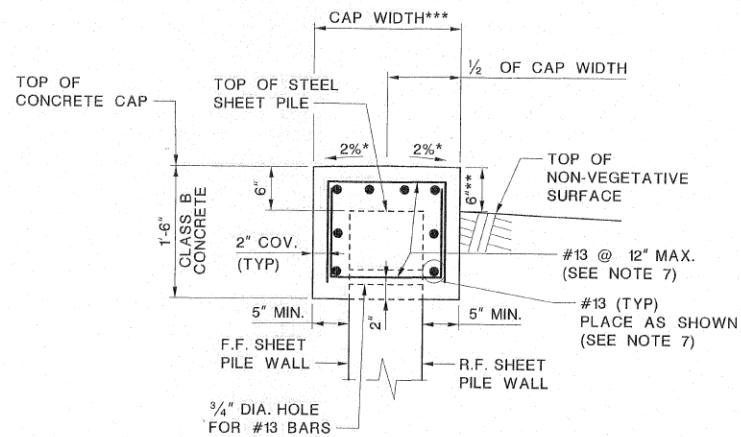
12
 122

NOTE:
 1. SEE RAILROAD GRADE CROSSING PLAN FOR GRADE CROSSING DETAILS
 2. REMOVAL OF FENCE TO BE PAID UNDER CLEARING SITE.
 B.M. 7 - REBAR WITH ALUMINIUM CAP A-LINE ROAD @ STA. 05+96.65, 21'06" RT., ELEV. 4.868
 B.M. 8 - REBAR WITH ALUMINIUM CAP A-LINE ROAD @ STA. 00+25.54, 10'06" LT., ELEV. 3.946

STATE	FEDERAL PROJECT NO.	SHEET	TOTAL SHEETS
N. J.		94	122



DRAINAGE DITCH DETAIL
SCALE: 1/2" = 1'-0"

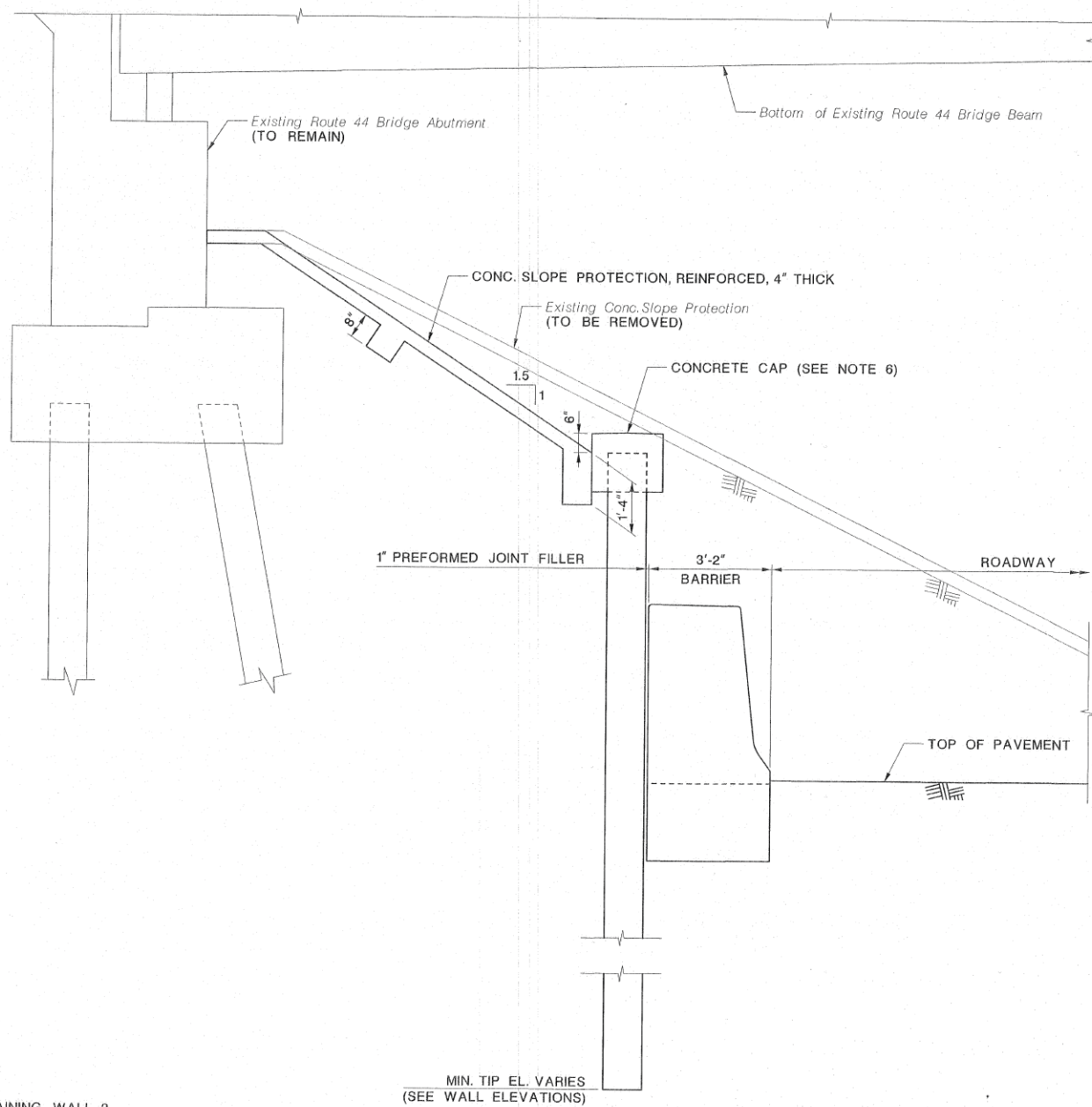


CONCRETE CAP DETAIL
SCALE: 1" = 1'-0"

* 0% FOR SECTION OF RETAINING WALL 2 THAT RUNS UNDER MOMENT SLAB 3

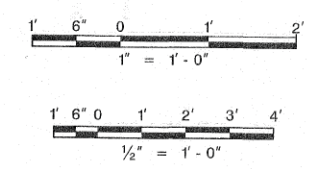
** 0% FOR SECTION OF RETAINING WALL 2 THAT RUNS UNDER MOMENT SLAB 3

*** FOR EACH RETAINING WALL, ESTABLISH CONCRETE CAP WIDTH BASED ON DEEPEST SHEET PILE SECTION USED FOR THAT WALL.



TYPICAL SECTION
SCALE: 1/2" = 1'-0"

- NOTES:
- FOR GENERAL NOTES, SEE SHEET NO. B-3.
 - FOR LEGEND OF ABBREVIATIONS, SEE SHEET NO. B-3.
 - FOR RETAINING WALL NO. 1 BARRIER DETAILS AND REINFORCEMENT, SEE SHEETS NO. B-24 AND B-25.
 - REMOVAL OF EXISTING CONCRETE SLOPE PROTECTION IS INCLUDED UNDER PAY ITEM "CLEARING SITE, BRIDGE (0805-154)".
 - SEE NJDOT STANDARD BRIDGE CONSTRUCTION DETAIL BCD-603-1 FOR CONCRETE SLOPE PROTECTION DETAILS.
 - CONTRACTOR TO FIELD VERIFY TOP OF CONCRETE CAP ELEVATIONS BASED ON PROPOSED CONCRETE SLOPE PROTECTION ELEVATIONS. MAKE NECESSARY ADJUSTMENTS TO TOP OF WALL ELEVATIONS IN ORDER TO PROVIDE 6" PROJECTION ABOVE FINISHED CONCRETE SLOPE PROTECTION.
 - THE CONTRACTOR MUST SUBMIT SHOP DRAWINGS FOR REINFORCEMENT BAR SCHEDULE PRIOR TO START OF CONCRETE CAP CONSTRUCTION. ALL BARS ARE TO BE EPOXY COATED.
 - FOR DRAINAGE DITCH LOCATIONS, SEE SHEET NO. B-4 AND B-7.



CONTROL SECTION	0805	JOB NO.	2206831
			2206833
DES. BY	L. C. TIMBER	CHK. BY	C. R. MARSTON
DWN. BY	L. C. TIMBER		
EST. BY	L. C. TIMBER	CHK. BY	C. R. MARSTON
SPECS. BY	B. T. ARLEDGE		

QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	CONTRACT QUANTITY
201006P	CLEARING SITE, BRIDGE (0805-154)	LS	LUMP SUM
603015P	CONCRETE SLOPE PROTECTION, REINFORCED, 4" THICK	SY	95
603036P	RIPRAP STONE CHANNEL PROTECTION, 12" THICK (D50=6")	SY	77

GLOUCESTER COUNTY IMPROVEMENT AUTHORITY

RETAINING WALL NO. 1 SECTIONS
ROUTE 44 TRUCK BYPASS AND DUPONT PORT ACCESS

McCORMICK TAYLOR, INC.
CERT. OF AUTHORIZ. NO. 24GA28044300
DATE: 12/3/19
CHANG H. CHUNG, P.E.
N.J. LIC. NO. 24GE04311400

BRIDGE SHEET NO. B-8 OF B-36

94
122

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REV. DATE: 11/20/2019
SCALE: 0.1" = 1'-0"

IN CHARGE OF *Chang H. Chung*



Photo 1 - Clearing site for proposed Route 44 Truck Bypass alignment. Staging of proposed Atlantic City Electric steel transmission poles.



Photo 2 - Construction of southbound ramp to Route 44. View from Route 44 overpass looking southbound.



Photo 3 - Completed Route 44 Truck Bypass looking northbound from Route 44. The southbound ramp to Route 44 is shown on the left. The northbound ramp to the port is shown on the right.



Photo 4 - Northbound ramp passing under first span of the existing Route 44 railroad overpass. Modified slope protection and steel sheeting retaining wall to the left. Conrail tracks to the right under the middle span.



Photo 5 - Proposed railroad grade crossing at connection to existing A-Line Road.

November 9, 2021

Mr. Joseph Fiordaliso, President
American Council of Engineering Companies of New Jersey
310 West State Street
Trenton, NJ 08618

Dear Mr. Fiordaliso,

We hereby grant permission to enter the Route 44 Truck Bypass and DuPont Port Access project in the 2022 ACECNJ Engineering Excellence Awards Program. We are pleased with the work of McCormick Taylor, Inc. and enthusiastically support the nomination of this important project.

The project involved the construction of a new 0.60-mile truck bypass roadway that ties into existing Route 44 in western Gibbstown, NJ. Without a truck bypass, traffic traveling to/from the DuPont site was forced to go through a residential neighborhood along Repauno Avenue to access Route 44 or Democrat Road. The bypass diverts truck traffic prior to Gibbstown's downtown corridor improving safety and quality of life for residents.

The design time of the project was significantly compressed by combining phases with each phase taking only six months. The further development of the port was tied to local approvals requiring that the bypass project be completed. The project unlocked the further development of the port, which is a vital to the economic success of the local community and region. We authorize the publication of the project and we agree that the project is substantially completed and in use.

Please contact me if you have any questions.

Sincerely,

A handwritten signature in blue ink, appearing to read 'Mario Christina', with a stylized flourish at the end.

Mario Christina
Project Manager
Gloucester County Improvement Authority



February 9, 2023

ASHE Northeast Region

Subject: ASHE National Project of the Year Application and Commitment Statement

Hello,

We are pleased to submit the Route 44 Truck Bypass and DuPont Port Access Project for the under \$20M category of the ASHE 2023 National Project of the Year. Enclosed you will find the following:

- Project Entry Form
- Project Narrative
- Five Photos
- Five Construction Drawings
- Verification of Substantial Completion

The submittal has been formatted in accordance with the application guidelines. With this application we are committed, if chosen for this award, to attend the awards luncheon at the 2023 National Conference.

Thank you for your consideration in reviewing ASHE NC-NJ, SNJ, and the Northeast Region's nomination for the ASHE National Project of the Year. Please don't hesitate to contact us with questions.

Sincerely,

A handwritten signature in black ink that reads "Anthony M. DiMaggio".

Anthony M. DiMaggio, PE, PTOE
Director, Transportation Engineering